


Memorandum

The City of Traverse City
Engineering Department



TO: Timothy J. Lodge, P.E., City Engineer

FROM: Jessica L. Cichowski, E.I.T., Engineering Technician 

DATE: February 26, 2014

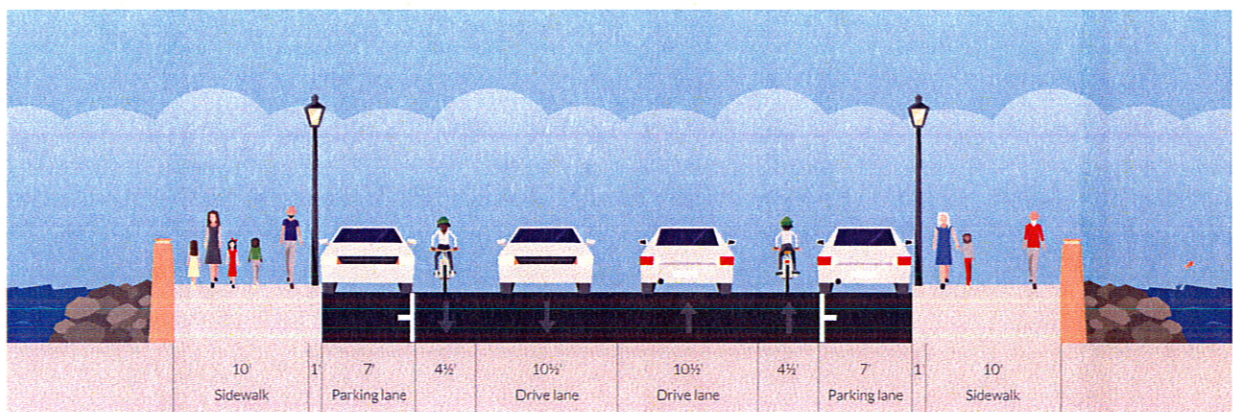
SUBJECT: Front St Bridge Planning Commission Memo

The Front St Bridge is scheduled for reconstruction and is to accommodate multiple modes of transportation as a Complete Street. This memo is a summary of the existing conditions on the applicable section of Front St and considerations for the design.

The community vision for Front St as outlined in the Corridors Master Plan is to encourage development and activity along the corridor through mixed land use and an emphasis on pedestrian facilities and to highlight the Boardman River. Based on this vision, these are the desirable design elements:

- Low target speed
- Wide sidewalks
- Street furniture and landscaping including benches and space for cafes, public space, etc.
- Pedestrian scaled lighting
- Street trees
- Bus stops with shelters
- Midblock crosswalks on long block sections
- Bicycle accommodations
- Gateway features/Access to River

The existing Front St characteristics are as follows:



- Right of way: 66 feet
- Two through-traffic lanes plus 11 foot sidewalks on each side
- On-street parking
- Average daily traffic (ADT): 13,702 vehicles per day
- Speed limit: 25 mph

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- Center turn lane: none
 - Transit: local route
 - Bicycle facilities: narrow lane adjacent to parking zone
 - No landscaping
 - Conventional street and safety lighting

Design Considerations:

Some design elements that should be considered also have potential trade-offs based on the available right of way width. These design elements and their trade-offs include:

- Travel lanes are generally 12' wide for this type of street and ADT but have been reduced to 10.5' to accommodate all modes.
- Accommodation of bicyclists is preferred to be at least 5' wide but have reduced to 4.5' to accommodate all modes.
- Parking lanes are preferred to be 8' wide but have been reduced to 7' to accommodate all modes.
- Sidewalks are preferred to be 12' to 18' wide but have been reduced to 11' to accommodate all modes and fit within the existing right of way.

The proposed geometric configuration at the bridge is the same as the existing configuration which accommodates pedestrians, bicyclists, and vehicular traffic. The project will include continuing the streetscape components of West Front Street and will be continued westerly as far as available funds allow. A sketch for the streetscapes continuing to Maple Street is attached for reference.

A boardwalk underneath the bridge along the west abutment wall is included with the bridge project and connections from the street level are being considered. The connections are dependent on obtaining right of way from the adjacent property owners for that purpose.

The bridge design will include aesthetic treatments to mimic the "arch" pattern of the existing bridge and a pedestrian scale railing with visibility to the water is desired. Pedestrian scale lighting along the street and under the bridge will be included in the project.

